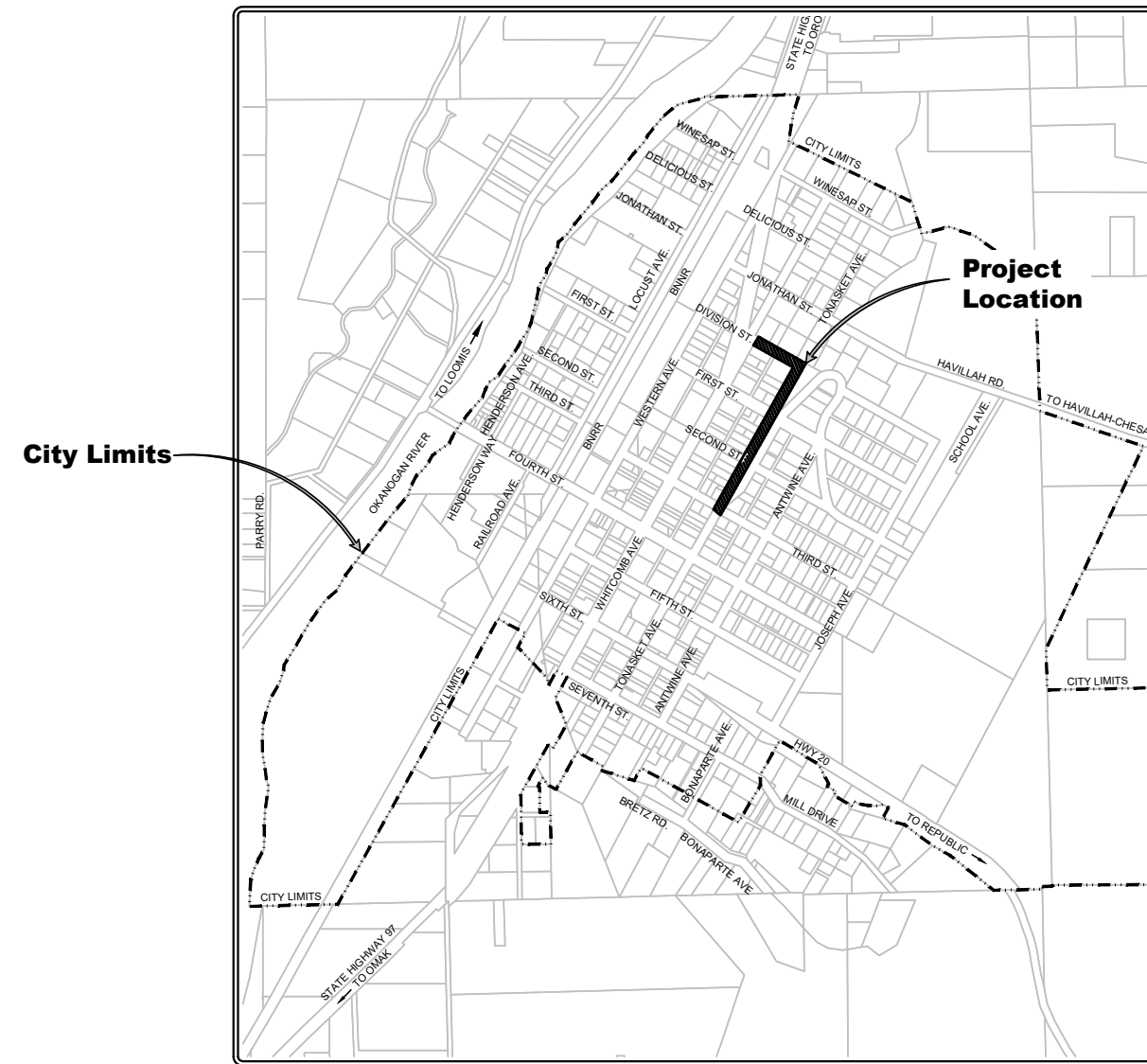
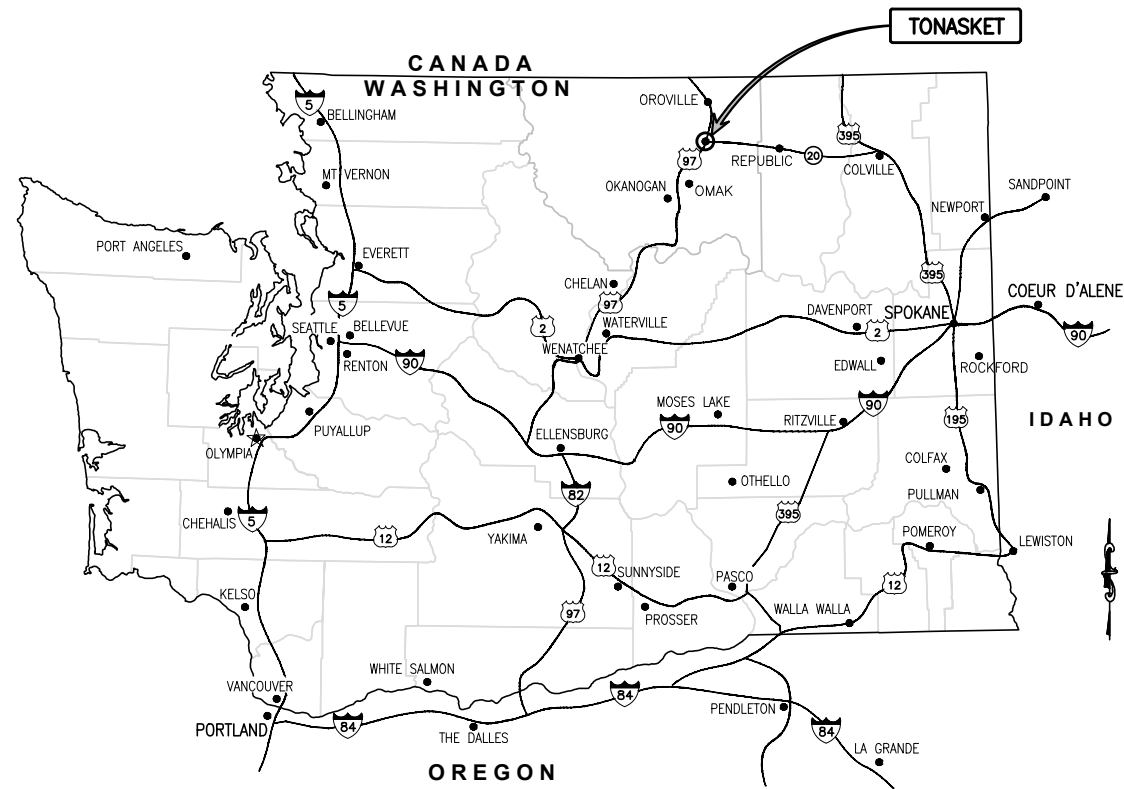


# CITY OF TONASKET, WASHINGTON

## TONASKET AVENUE & DIVISION STREET REHABILITATION PROJECT

TRANSPORTATION IMPROVEMENT BOARD FUNDS PROJECT NO.: 2-E-885(003)-1

May 2020



### CITY OF TONASKET OFFICIALS

MAYOR

*Dennis Brown*

CITY SUPERINTENDENT

*Darren Johnson*

CITY CLERK/TREASURER

*Alice Attwood*

CITY ATTORNEY

*Michael D. Howe*



COUNCIL

*Christa Levine*

*Jeff McMillan*

*Jill Ritter*

*Alisa Weddle*

*Marylou Kriner*

BUILDING OFFICIAL/PERMIT ADMINISTRATOR

*Okanogan County Building Department*

CITY OF TONASKET

N.T.S.

BID SET

582603 Sht 01 (Cover)



**CITY OF TONASKET, WASHINGTON**  
TONASKET AVE. & DIVISION ST. REHABILITATION

COVER SHEET, PROJECT LOCATION

SHEET  
**01**  
OF 07

# ABBREVIATIONS

PIPE	AC CONC CMP CI DI FM GS PVC RCP STL	ASBESTOS CONCRETE CONCRETE CORRUGATED METAL PIPE CAST IRON DUCTILE IRON FORCE MAIN GALVANIZED STEEL POLYVINYL CHLORIDE PIPE REINFORCED CONCRETE PIPE STEEL PIPE	MISC	A.C. B C CB C.F.S. CL C.O. CONC. CSBC CSTC C.Y. D DEC. DET. DIA. DIM. DR DW DWG E EG ELEV. EP EXIST. FF FG FH FT. GR HP HWY I.E. IN. LF	ASPHALT CONCRETE BASEMENT CONCRETE CATCH BASIN CUBIC FEET PER SECOND CENTERLINE CLEAN OUT CONCRETE CRUSHED SURFACING BASE COURSE CRUSHED SURFACING TOP COURSE CUBIC YARD DIRT DECIDUOUS DETAIL DIAMETER DIMENSION DRAIN PIPE DRYWELL DRAWING EAST EXISTING GRADE ELEVATION EDGE OF PAVEMENT EXISTING FINISH FLOOR FINISH GRADE FIRE HYDRANT FOOT OR FEET GRADE HIGH PRESSURE HIGHWAY INVERT ELEVATION INCHES LINEAL FEET	MAX. MH MIN. N N.I.C. NO. NTS O.C. P PP P.S.I. Q.C. Q.D. REV. REQ'D RD. R.R. R/W S S= SD SHT. SL SS ST. STA STD. SW SY T.B.M. TESC T.O.P. T.O.W. TYP. W	MAXIMUM MANHOLE MINIMUM NORTH NOT IN CONTRACT NUMBER NOT TO SCALE ON CENTER PAVED POWER POLE POUNDS PER SQUARE INCH FULL PIPE CAPACITY DESIGN FLOW REVISED REQUIRED ROAD RAILROAD RIGHT-OF-WAY SOUTH SLOPE STORM DRAIN SHEET SURVEY LINE SANITARY SEWER STREET STATION STANDARD SIDEWALK SQUARE YARD TEMPORARY BENCH MARK TEMPORARY EROSION SEDIMENT CONTROL TOP OF PIPE TOP OF WALL TYPICAL WEST
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# GENERAL NOTES

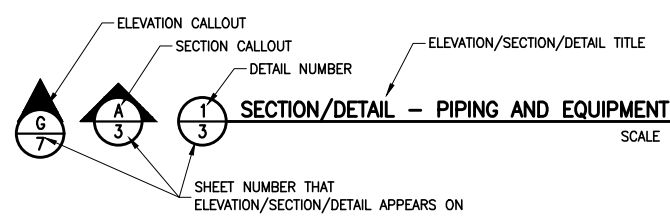
- ALL MATERIALS, WORKMANSHIP, AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE 2020 WASHINGTON STATE DEPARTMENT OF TRANSPORTATION (SOT) STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION (M41-10) AS AMENDED BY THE PROJECT SPECIAL PROVISIONS AND AS SHOWN ON THE PLANS.
- WHERE KNOWN, EXISTING UTILITIES ARE SHOWN ON THE PLANS FOR THE CONVENIENCE OF THE CONTRACTOR. LOCATIONS SHOWN ARE APPROXIMATE AND DEPTHS ARE NOT KNOWN. OTHER UTILITIES MAY EXIST IN THE WORK AREAS THAT ARE NOT SHOWN ON THE DRAWINGS. THE CONTRACTOR SHALL VERIFY THE EXISTENCE, LOCATIONS, NATURE AND DEPTH OF THE EXISTING UTILITIES IN THE WORK AREA PRIOR TO CONSTRUCTION BY CONTACTING ALL UTILITY OWNERS. OVERHEAD UTILITIES ARE GENERALLY NOT SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT AND REPAIR ALL UTILITIES ENCOUNTERED DURING CONSTRUCTION.
- WHERE THE POTENTIAL FOR CONFLICT WITH AN EXISTING UTILITY CROSSING EXISTS, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO EXPOSE, AS NECESSARY, THE EXISTING UTILITY ADEQUATELY IN ADVANCE OF THE WORK SO THAT AN ADJUSTMENT TO GRADE CAN BE PROPOSED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER IF NECESSARY.
- THE CONTRACTOR IS RESPONSIBLE TO MAINTAIN ACCURATE RED LINE DRAWINGS OF THE AS-BUILT PROJECT FEATURES FOR THE DURATION OF THE PROJECT. RED LINE DRAWINGS SHALL BE MAINTAINED ON SITE AT ALL TIMES AND MADE AVAILABLE TO THE ENGINEER AND CONTRACTING AGENCY UPON REQUEST.
- THE CONTRACTOR IS RESPONSIBLE TO OBTAIN ALL NECESSARY PERMITS FROM THE CONTRACTING AGENCY AND OTHER AGENCIES WITH JURISDICTION PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION STAGING AND STORAGE AREAS WITH THE CONTRACTING AGENCY. THE CONTRACTOR SHALL NOT HAVE ACCESS TO, OR USE OF, PRIVATE PROPERTY WITHOUT PRIOR AGREEMENT WITH THE PROPERTY OWNER. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION OF THIS AGREEMENT IN ACCORDANCE WITH SECTION 1-07.16(1)C.
- RIGHT-OF-WAY AND PROPERTY BOUNDARY LINES WHERE SHOWN ON THE DRAWINGS ARE APPROXIMATE.
- CONTRACTOR IS RESPONSIBLE TO VERIFY EXISTING ELEVATIONS OF THE ROADWAY, CURBING, AND OTHER EXISTING FEATURES PERTINENT TO THE WORK ADEQUATELY IN ADVANCE. ANY CHANGES SHALL BE REPORTED TO THE ENGINEER SO ADJUSTMENTS CAN BE MADE, IF NEEDED.
- EXCEPT AS NOTED FOR REMOVAL ON THE PLANS, THE CONTRACTOR SHALL PROTECT TREES. WHERE INTERFERENCE WITH TREES CANNOT BE AVOIDED, THE CONTRACTOR SHALL NEATLY TRIM BRANCHES. TREES DAMAGED AS A RESULT OF THE CONTRACTOR'S (OR SUBCONTRACTOR'S) ACTIONS SHALL BE REPAIRED TO THE OWNER'S SATISFACTION AT NO ADDITIONAL COST TO THE OWNER.
- LOCATIONS OF ROADWAYS, BUILDINGS, AND OTHER FEATURES SHOWN ON THE DRAWINGS ARE APPROXIMATE.

# SHEET INDEX

Sheet Number	Sheet Title
01	COVER SHEET, PROJECT LOCATION
02	LEGEND, ABBREVIATIONS, NOTES, SHEET INDEX
03	TONASKET AVE. 10+00 TO STA 15+50
04	TONASKET AVE. STA 15+50 TO STA 21+00
05	DIVISION ST. 10+00 TO 13+25
06	TYPICAL ROAD SECTIONS
07	TRAFFIC CONTROL PLAN

# LEGEND

DESCRIPTION	EXISTING	PROPOSED	DESCRIPTION	EXISTING	PROPOSED
<b>SURVEY</b>			<b>SURFACE</b>		
CONTROL NUMBER	▲12		ASPHALT		
MONUMENT (SURFACE)	●Mon.		BUILDING		
<b>STORM SEWER</b>			GRAVEL/DIRT		
CATCH BASIN	□C.B.	■	CONCRETE		
DRYWELL	⊙DW	⊙	TREE/SHRUB		
MANHOLE	⊙MH	⊙	CURB AND GUTTER		
CULVERT	≡≡≡	≡≡≡	SIDEWALK		
<b>SANITARY SEWER</b>			CREEK/DITCH		
MANHOLE	⊙MH	⊙	FENCE		
<b>WATER</b>			<b>SURVEY</b>		
VALVE	⊙WV	⊙WV	SURVEY CENTERLINE		
FIRE HYDRANT	⊙FH	⊙FH	CONTOUR		
METER	⊙WM	⊙	CONTOUR (INDEX)		
<b>UTILITIES</b>			PROPERTY LINE		
UTILITY POLE	○PP	○	RIGHT-OF-WAY		
UTILITY POLE ANCHOR	←UPA	←			
TELEPHONE PEDESTAL	⊙TR	⊙			
STORM DRAINAGE	— 8"SD(PVC) —	— 8"SD(PVC) —			
GRAVITY SEWER	— 8"SS(PVC) —	— 8"SS(PVC) —			
WATER	— 8"W(PVC) —	— 8"W(PVC) —			
ORVILLE-TONASKET IRR.	— 2"W(PVC) — OTID	— 2"W(PVC) — OTID			
TELEPHONE (BURIED)	— —BT— —	— —BT— —			
POWER (BURIED)	— —BP— —	— —BP— —			
<b>TRAFFIC SIGN</b>					
SIGN	○S	○			



# DEMOLITION AND SURFACE REPLACEMENT NOTES

- CONTRACTOR SHALL MAKE NECESSARY SITE VISIT(S) AND COMPLETE ANY OTHER INVESTIGATIONS AND TASKS NECESSARY TO DETERMINE EXISTING CONDITIONS AND DEMOLITION REQUIREMENTS NECESSARY TO CONSTRUCT THE IMPROVEMENTS HEREIN. SEE SECTION 1-02.4.
- ALL MATERIALS REMOVED BY THE CONTRACTOR SHALL BE DISPOSED OF OFF SITE IN ACCORDANCE WITH ALL APPLICABLE STATE AND LOCAL LAWS, AT NO COST TO THE CONTRACTING AGENCY.
- THE CONTRACTOR IS RESPONSIBLE TO PROTECT IN PLACE ALL ITEMS NOT SHOWN TO BE REMOVED. ITEMS DAMAGED BY THE CONTRACTOR SHALL BE REPLACED OR REPAIRED TO THE OWNER'S SATISFACTION BY THE CONTRACTOR AT NO COST TO THE CONTRACTING AGENCY.
- DEMOLITION LIMITS SHOWN ARE FOR BIDDING PURPOSES ONLY. CONTRACTOR SHALL COORDINATE DEMOLITION LIMITS AND SAWCUT LOCATIONS WITH THE ENGINEER IN THE FIELD, PULVERIZATION LIMITS SHALL BE TO THE NEAT LINES SHOWN ON THE PLANS UNLESS OTHERWISE APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MARK SAWCUT LOCATIONS FOR APPROVAL BY ENGINEER PRIOR TO SAWCUTTING, PULVERIZATION, AND REMOVAL OF EXISTING CURBS.
- ALL PAVEMENT TO BE REMOVED SHALL BE SAWCUT OR WHEEL CUT PRIOR TO REMOVAL. AS DIRECTED IN THE FIELD BY THE ENGINEER ALL JAGGED PAVING EDGES SHALL BE SAWCUT PRIOR TO FINAL ASPHALT RESTORATION. ALL CUTS SHALL BE PARALLEL OR PERPENDICULAR TO THE ROADWAY.
- EXCAVATIONS THAT UNDERCUT EXISTING PAVEMENT SHALL BE REPLACED AT THE DIRECTION OF THE CITY OF TONASKET PUBLIC WORKS DEPARTMENT. THE CONTRACTOR SHALL PROTECT EXISTING PAVEMENT SURFACES AND CURB AND GUTTER FROM EQUIPMENT DAMAGE.
- CASTINGS FOR MONUMENTS, WATER VALVES, VAULTS, AND OTHER SIMILAR INSTALLATIONS THAT HAVE BEEN COVERED IN ASPHALT MATERIAL SHALL BE CLEANED BY THE CONTRACTOR TO THE SATISFACTION OF THE CONTRACTING AGENCY. PAYMENT FOR CLEANING SHALL BE INCLUDED IN THE COST OF ADJUSTMENT.
- CONTRACTOR SHALL RE-ESTABLISH THE ROADWAYS DISTURBED BY ITS OPERATIONS TO THEIR ORIGINAL GRADES, LOCATIONS, AND WIDTH UNLESS OTHERWISE SHOWN ON THE DRAWINGS OR DIRECTED IN THE FIELD BY THE ENGINEER. THE COMPLETED SURFACE OF ALL COURSES SHALL BE OF UNIFORM TEXTURE, SMOOTH UNIFORM CROWN AND GRADE IN ACCORDANCE WITH SECTION 5-04.3 (13) OF THE WSDOT STANDARD SPECIFICATIONS.
- FINISHED GRADE CURB AND ASPHALT PROFILES TO BE SMOOTH AND FREE OF LOCAL DEPRESSIONS OR ABRUPT GRADE CHANGES SUCH THAT PUDDLING OF WATER DOES NOT OCCUR. UTILIZE PULVERIZED MATERIAL AS NECESSARY TO REPAIR SUNKEN AREAS (TYPICAL).
- MATCH EXISTING GRADE AT ALL LOCATIONS WHERE PROPOSED FEATURES MEET EXISTING FEATURES (TYPICAL).
- THE CONTRACTOR SHALL PLACE TEMPORARY SURFACING AT ALL EXISTING PRIVATE PROPERTY ACCESS POINTS TO THE SAME ELEVATION AS THE EXISTING ROADWAY BY THE END OF EACH DAY AFTER BACKFILL UNTIL PERMANENT SURFACING CAN BE PLACED. THE TEMPORARY SURFACE CAN BE COLD MIX ASPHALT, STEEL PLATES OR GRAVEL AT THE CONTRACTORS OPTION. THE CONTRACTOR SHALL MAINTAIN THE TEMPORARY SURFACING UNTIL THE PERMANENT SURFACING IS PLACED.
- THE CONTRACTOR SHALL PROVIDE SHOULDER RESTORATION WHERE DISTURBED BY CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL SEED LAWN AREAS DISTURBED BY CONSTRUCTION ACTIVITIES.
- CONTRACTOR SHALL RESTORE ANY LANDSCAPING, FENCES, OR IRRIGATION SYSTEMS DISTURBED, REMOVED, OR DAMAGED DURING CONSTRUCTION. RESTORATION WILL MEAN TO REPLACE WITH LIKE MATERIALS SUCH AS, BUT NOT LIMITED TO, SOD, BARK, OR ROCK MULCH. REPAIRED IRRIGATION SYSTEMS SHALL BE DEMONSTRATED OPERATIONAL BY THE CONTRACTOR PRIOR TO ACCEPTANCE. LANDSCAPE, FENCES, AND IRRIGATION RESTORATION SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO PAYMENT SHALL BE MADE. ITEMS DAMAGED BY THE CONTRACTORS OPERATIONS SHALL BE RESTORED TO THE SATISFACTION OF THE OWNER. AREAS OF RESTORATION OR REPAIR MAY OR MAY NOT BE INDICATED ON THE PLANS. THE CONTRACTOR IS RESPONSIBLE TO IDENTIFY ALL SUCH AREAS PRIOR TO AND INCLUDE IN THE BID. SEPARATE PAYMENT WILL NOT BE MADE.

# TESC NOTES

- EROSION CONTROL IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. EROSION CONTROL AND WATER POLLUTION CONTROL SHALL BE IN ACCORDANCE WITH WSDOT STANDARD SPECIFICATIONS.
- CONTRACTOR SHALL MAINTAIN AN OPERABLE WATER TRUCK ON SITE TO PROVIDE DUST CONTROL TO THE SATISFACTION OF THE CONTRACTING AGENCY.
- SILT FENCE SHALL BE IN ACCORDANCE WITH WSDOT STANDARD PLAN 1-30.15-02.
- THE CONTRACTOR IS RESPONSIBLE TO PROVIDE INLET PROTECTION AT ALL EXISTING DRYWELLS AND DRAINAGE STRUCTURES WITHIN THE PROJECT AREA AND IMMEDIATELY DOWNSTREAM. INLET PROTECTION SHALL MEET THE REQUIREMENTS OF WSDOT STANDARD PLAN 1-40.20-00 OR AS REQUIRED BY THE INDIVIDUAL BMP. THE CONTRACTOR IS ALSO RESPONSIBLE FOR THE MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE TESC FACILITIES UNTIL ALL CONSTRUCTION IS COMPLETED AND VEGETATION IS ESTABLISHED, AND APPROVED BY THE ENGINEER.
- THE TESC FACILITIES SHOWN ON THE PLANS MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES AND IN SUCH A MANNER AS TO ENSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DO NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
- THE TESC FACILITIES SHOWN ON THE PLANS ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE TESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT-LADEN WATER DO NOT LEAVE THE SITE.
- THE TESC FACILITIES SHALL BE INSPECTED DAILY BY THE CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTION. THE CONTRACTOR SHALL CLEAN ALL EXISTING DRAINAGE STRUCTURES SUBJECT TO SOIL INFILTRATION AS A RESULT OF THE CONTRACTORS WORK AND SUCH CLEANING SHALL BE INCIDENTAL TO OTHER BID ITEMS
- AT NO TIME SHALL MORE THAN ONE-FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A TRAPPED CATCH BASIN.

# SIGN AND PAVING MARKING NOTES

- GROUND MOUNTED SIGN PLACEMENT SHALL BE IN ACCORDANCE WITH WSDOT STANDARD PLAN G-20.10-02.
- STEEL SIGN SUPPORTS SHALL BE TYPE ST-4 IN ACCORDANCE WITH WSDOT STANDARD PLAN G-24.50-05.
- ROAD SIGNS SHALL MEET THE REQUIREMENTS OF THE 2009 MUTCD.
- SIGN LOCATIONS SHOWN IN THE PLANS ARE APPROXIMATE. CONTRACTOR IS TO COORDINATE LOCATION WITH ENGINEER IN THE FIELD.
- CONTRACTOR SHALL VERIFY SIGN SUPPORT LENGTHS PRIOR TO ORDERING.
- CONTRACTOR SHALL SALVAGE ALL EXISTING SIGNS AND HARDWARE FOR REUSE AND RETURN SIGN SUPPORT TO CONTRACTING AGENCY. CONTRACTOR SHALL PROVIDE NEW SIGN SUPPORT POST AND FOUNDATION.



ONE INCH  
AT FULL SIZE  
IF NOT ONE INCH  
SCALE ACCORDINGLY

582603 Sht 02 (Legend)

5-01-20



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DRAWN: VAC/TVP  
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PROJ. NO.: 58-26-03  
DATE: 5/01/2020

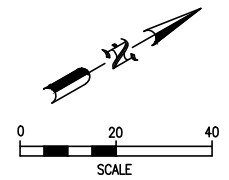
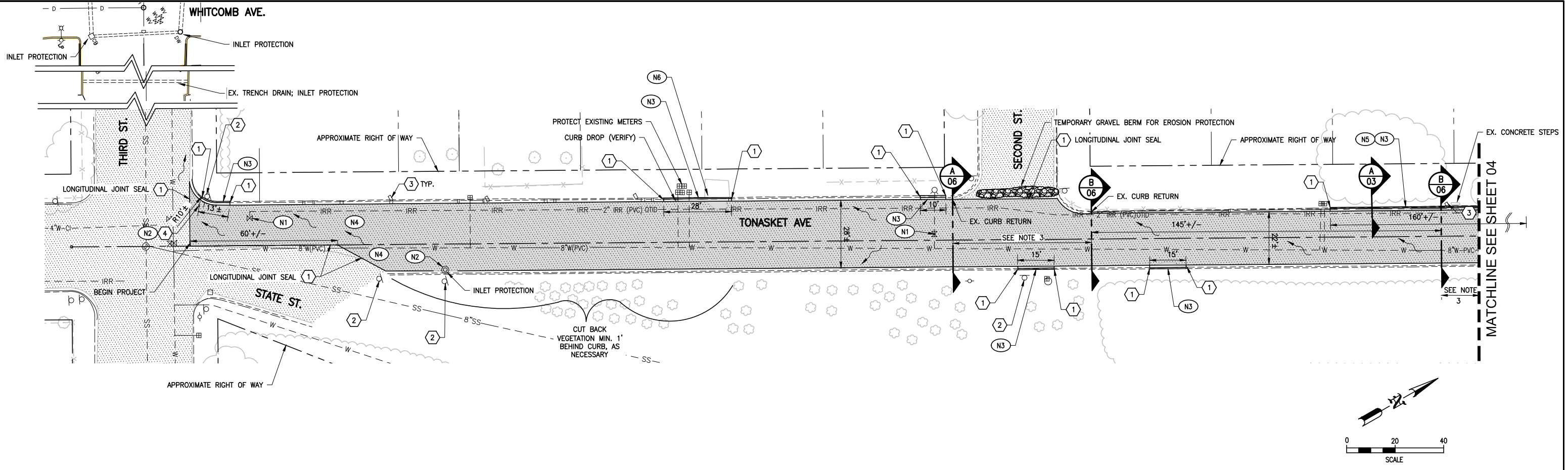


**CITY OF TONASKET, WASHINGTON**  
TONASKET AVE. & DIVISION ST. REHABILITATION PROJECT

LEGEND, ABBREVIATIONS, NOTES, SHEET INDEX

SHEET  
**02**  
OF 07

NO.	DATE	BY	CKD.	APP.	REVISIONS



**NOTES**

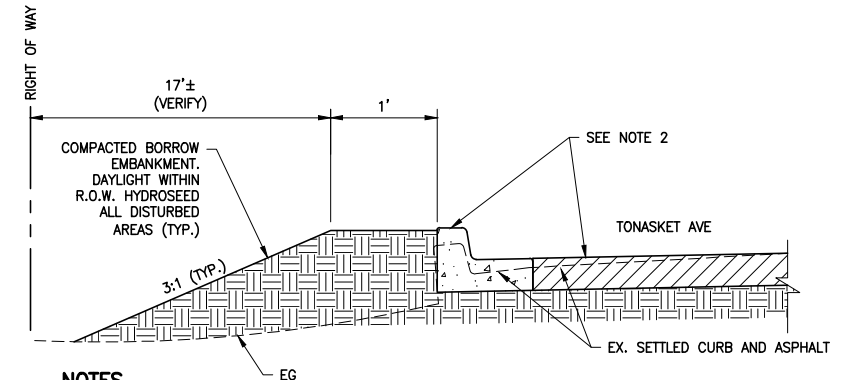
- CONTRACTOR SHALL PULVERIZE EXISTING PAVEMENT FOR THE FULL WIDTH BETWEEN CURBS OF EXISTING PAVED ROADWAY TO THE DEPTH SHOWN ON THE ROAD SECTIONS. PULVERIZATION AT INTERSECTIONS TO EXTEND TO THE SAWCUT LINES APPROVED BY THE ENGINEER. EXISTING ROADWAY TO BE PULVERIZED, RESHAPED, AND GRADED TO CONFORM WITH THE DETAILS PROVIDED IN THESE PLANS.
- DIMENSIONS SHOWN FOR THE CONVENIENCE OF THE CONTRACTOR. THE MAJORITY OF THE PROJECT IS LOCATED BETWEEN THE CONCRETE CURB AND GUTTER.
- CONTRACTOR TO CREATE A SMOOTH TRANSITION BETWEEN SHOWN ROAD SECTIONS. MAINTAIN EXISTING DRAINAGE PATHWAYS.
- CURB RETURN RADII SHOWN ARE APPROXIMATE AND AT FACE OF CURB. (TYP.)
- PAYMENT TO ADJUST DRYWELL RIM TO FINISH GRADE WILL BE MADE UNDER ITEM "ADJUST MANHOLE OR CATCH BASIN".

**KEY NOTES**

- (N1) ADJUST VALVE BOX (TYPICAL).
- (N2) ADJUST DRYWELL OR CATCH BASIN RIM (TYPICAL).
- (N3) REPLACE CEMENT CONCRETE TRAFFIC CURB AND GUTTER PER WSDOT STANDARD PLAN F-10.12-03. SEE DETAIL C, SHEET 06.
- (N4) CREATE SMOOTH TRANSITION FROM TYPICAL ROAD SECTION TO EXISTING GRADES. GRADE TO MAINTAIN EXISTING DRAINAGE PATTERNS. SEE FLOW ARROWS
- (N5) CURB & GUTTER AND GRADE REHABILITATION AREA, TOTAL LENGTH APPROXIMATELY 160 FEET. SEE CROSS-SECTION DETAIL A, THIS SHEET.
- (N6) DRIVEWAY TRANSITION. PROVIDE 4" CSTC FROM BACK OF CURB TO UNDISTURBED DRIVEWAY. CREATE SMOOTH TRANSITION FROM BACK OF CURB TO EXISTING GRADE.

**DEMOLITION KEY NOTES**

- ① SAWCUT (TYPICAL)
- ② REMOVE AND RESET SIGN, POST, AND BASE, AS NECESSARY
- ③ REMOVE AND RESET MAILBOX, POST, AND BASE, AS NECESSARY
- ④ VERIFY, UNCOVER EXISTING INLET



**NOTES**

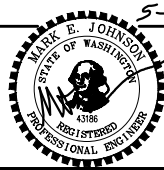
- SECTION IS TO ILLUSTRATE SUNKEN OR SETTLED AREA OF ROADWAY. INTENT IS TO REPLACE CURB & GUTTER AND RE-ESTABLISH ORIGINAL CURB GRADES, ROAD GRADES, AND SHOULDER IN THIS AREA AS MUCH AS POSSIBLE.
- SEE SECTION DETAILS, SHEET 6, FOR MORE INFORMATION ON CURB & GUTTER AND PAVEMENT SECTION.
- MINIMIZE WIDTH OF EMBANKMENT DAYLIGHT SLOPE WHERE POSSIBLE. COORDINATE WITH ENGINEER IN THE FIELD.
- PROTECT PUBLIC UTILITIES IN PLACE AND COORDINATE WITH CITY AND ADJACENT PROPERTY OWNER PRIOR TO BEGINNING WORK IN THAT AREA.
- LENGTH OF REPAIR SHOWN IS FOR BIDDING PURPOSES. COORDINATE ACTUAL EXTENTS OF SHOULDER IMPROVEMENTS WITH ENGINEER IN THE FIELD.
- PROTECT AND PRESERVE EXISTING CONCRETE STEPS IN PLACE.
- LEFT TOP-OF CURB ELEVATION TYPICALLY TO BE 0.20'± ABOVE CENTER LINE GRADE AT CROSS SECTIONS.

**A 03 CURB AND GRADE REHABILITATION AREA SHOULDER SECTION** N.T.S.

ONE INCH  
AT FULL SIZE  
IF NOT ONE INCH  
SCALE ACCORDINGLY

582603 Sht 03-05 (Plan)

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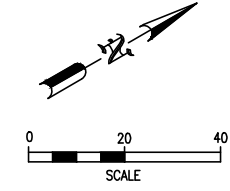
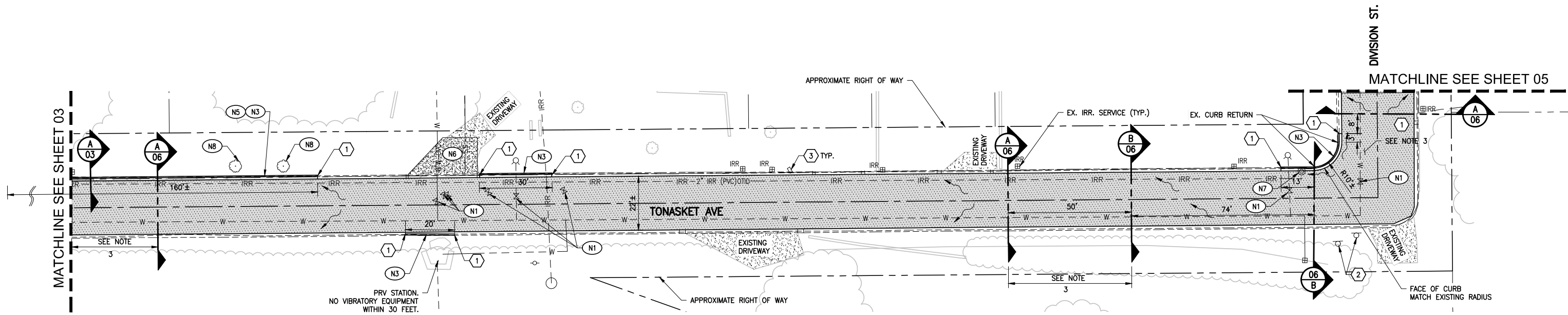


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TONASKET AVE. & DIVISION ST. REHABILITATION PROJECT  
TONASKET AVE. 10+00 TO STA 15+50

SHEET  
**03**  
OF 07



**NOTES**

1. CONTRACTOR SHALL PULVERIZE EXISTING PAVEMENT FOR THE FULL WIDTH BETWEEN CURBS OF EXISTING PAVED ROADWAY TO THE DEPTH SHOWN ON THE ROAD SECTIONS. PULVERIZATION AT INTERSECTIONS TO EXTEND TO THE SAWCUT LINES APPROVED BY THE ENGINEER. EXISTING ROADWAY TO BE PULVERIZED, RESHAPED, AND GRADED TO CONFORM WITH THE DETAILS PROVIDED IN THESE PLANS.
2. DIMENSIONS SHOWN FOR THE CONVENIENCE OF THE CONTRACTOR. THE MAJORITY OF THE PROJECT IS LOCATED BETWEEN THE CONCRETE CURB AND GUTTER.
3. CONTRACTOR TO CREATE A SMOOTH TRANSITION BETWEEN SHOWN ROAD SECTIONS. MAINTAIN EXISTING DRAINAGE PATHWAYS.
4. CURB RETURN RADII SHOWN ARE APPROXIMATE AND AT FACE OF CURB. (TYP.)
5. COORDINATE REMOVAL OF EXISTING TREES AND VEGETATION WITH CITY PRIOR TO MOBILIZING FOR THE WORK. CONTRACTOR SHALL STAKE LIMITS OF HIS EXCAVATION AND MARK TREES THAT NEED TO BE REMOVED. TREES SHALL NOT BE REMOVED UNTIL APPROVED BY THE OWNER. TREES TO BE REMOVED SHALL BE REMOVED AND DISPOSED OF OFFSITE. TRUNKS SHALL BE GROUND DOWN TO MINIMUM 6-INCHES BELOW GRADE OR REMOVED ENTIRELY. SOIL SHALL BE PLACED TO PROVIDE A SMOOTH FINISHED SURFACE.

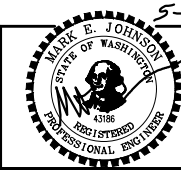
**KEY NOTES**

- (N1) ADJUST VALVE BOX (TYPICAL).
- (N3) REPLACE CEMENT CONCRETE TRAFFIC CURB AND GUTTER PER WSDOT STANDARD PLAN F-10.12-03. SEE DETAIL C, SHEET 06.
- (N5) CURB & GUTTER AND GRADE REHABILITATION AREA. TOTAL LENGTH APPROXIMATELY 160 FEET. SEE CROSS-SECTION DETAIL A, SHEET 03.
- (N6) DRIVEWAY TRANSITION. PROVIDE 4" CSTC FROM BACK OF CURB TO UNDISTURBED DRIVEWAY. CREATE SMOOTH TRANSITION FROM BACK OF CURB TO EXISTING GRADE.
- (N7) ADJUST MANHOLE RIM (UNKNOWN TYPE).
- (N8) REMOVE EXISTING TREES ENCROACHING UPON CURB/PAVEMENT FOR PROPOSED IMPROVEMENTS. COORDINATE WITH CITY PRIOR TO REMOVAL. SEE NOTE 5.

**DEMOLITION KEY NOTES**

- (1) SAWCUT (TYPICAL)
- (2) REMOVE AND RESET SIGN, POST, AND BASE, AS NECESSARY
- (3) REMOVE AND RESET MAILBOX, POST, AND BASE, AS NECESSARY

582603 Sht 03-05 (Plan)



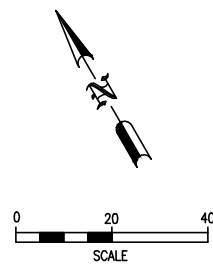
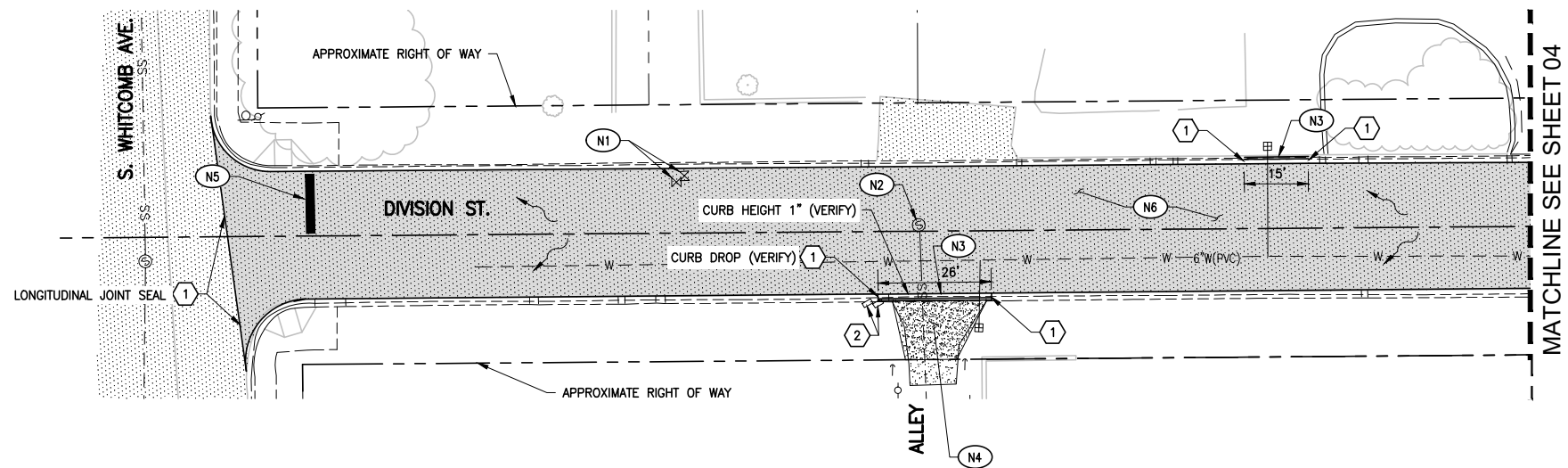
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 TONASKET AVE. STA 15+50 TO STA 21+00

ONE INCH  
 AT FULL SIZE  
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**NOTES**

1. CONTRACTOR SHALL PULVERIZE EXISTING PAVEMENT FOR THE FULL WIDTH BETWEEN CURBS OF EXISTING PAVED ROADWAY TO THE DEPTH SHOWN ON THE ROAD SECTIONS. PULVERIZATION AT INTERSECTIONS TO EXTEND TO THE SAWCUT LINES APPROVED BY THE ENGINEER. EXISTING ROADWAY TO BE PULVERIZED, RESHAPED, AND GRADED TO CONFORM WITH THE DETAILS PROVIDED IN THESE PLANS.
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**KEY NOTES**

- (N1) ADJUST VALVE BOX (TYPICAL).
- (N2) ADJUST MANHOLE RIM (TYPICAL).
- (N3) REPLACE CEMENT CONCRETE TRAFFIC CURB AND GUTTER PER WSDOT STANDARD PLAN F-10.12-03. SEE DETAIL C, SHEET 06.
- (N4) DRIVEWAY TRANSITION. PROVIDE 4" CSTC FROM BACK OF CURB TO UNDISTURBED DRIVEWAY. CREATE SMOOTH TRANSITION FROM BACK OF CURB TO EXISTING GRADE.
- (N5) STOP BAR.
- (N6) DIVISION STREET ROAD CROSS SECTION TO BE CROWNED, PER CROSS SECTION DETAIL A, SHEET 06.

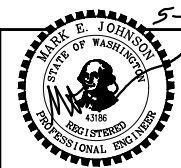
**DEMOLITION KEY NOTES**

- (1) SAWCUT (TYPICAL)
- (2) REMOVE AND RESET MAILBOX, POST, AND BASE, AS NECESSARY

582603 Sht 03-05 (Plan)

ONE INCH  
AT FULL SIZE  
IF NOT ONE INCH  
SCALE ACCORDINGLY

NO.	DATE	BY	CHKD.	APP.	REVISIONS



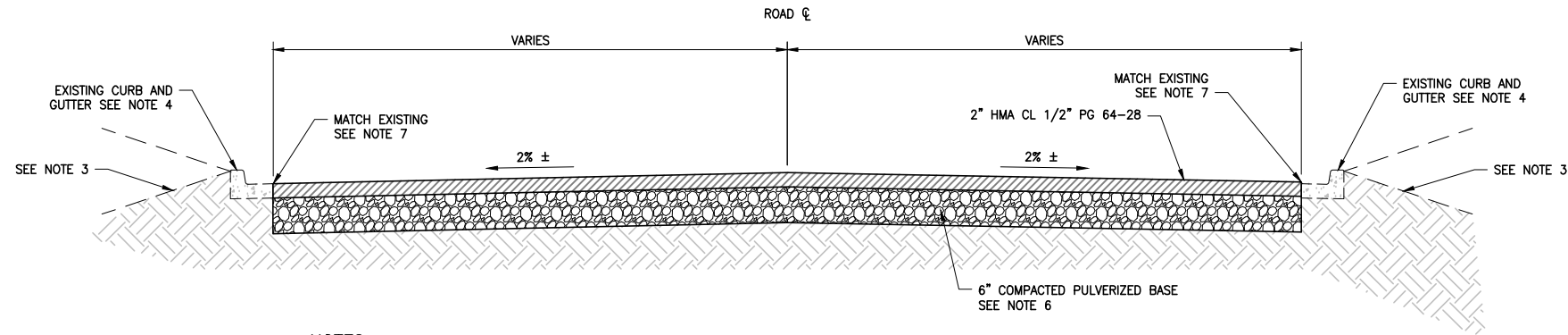
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DESIGNED: VAC/MEJ  
DRAWN: VAC/TVP  
CHECKED: VAC/TVP  
APPROVED: VAC/TVP  
PROJ. NO.: 58-26-03  
DATE: 5/01/2020



**CITY OF TONASKET, WASHINGTON**  
TONASKET AVE. & DIVISION ST. REHABILITATION PROJECT

DIVISION ST. 10+00 TO 13+25

SHEET  
**05**  
OF 07

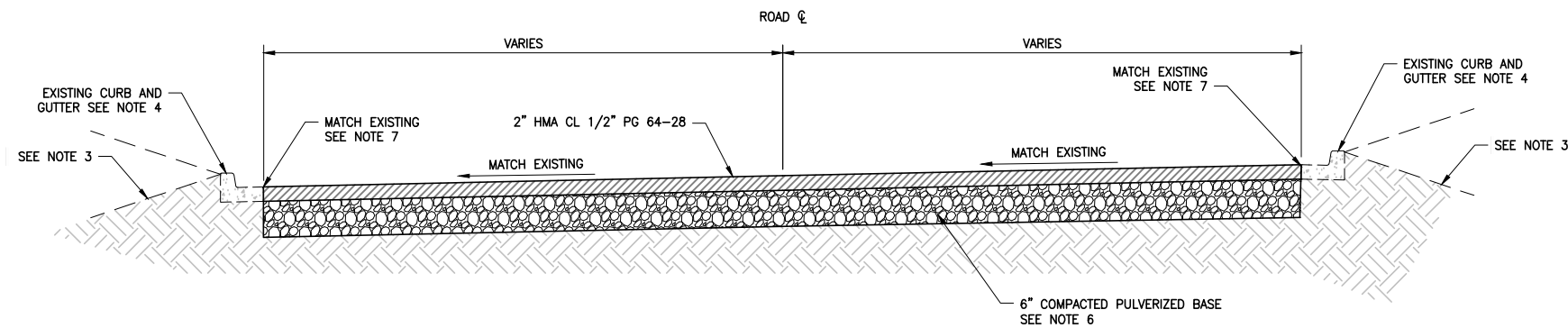


**NOTES**

1. ALL DEPTH AND THICKNESS SHOWN ARE COMPACTED DEPTH AND THICKNESS.
2. MATCH EXISTING LANE WIDTHS.
3. PROVIDE COMPACTED COMMON BORROW EMBANKMENT WHERE BACK OF EXISTING CURB IS REVEALED. SEE CURB AND GUTTER SECTION DETAIL FOR MORE INFORMATION, THIS SHEET. COORDINATE LOCATIONS WITH ENGINEER IN THE FIELD. (TYP.)
4. SEE PLAN VIEW FOR ANTICIPATED CURB AND GUTTER REPLACEMENT LOCATIONS (USED FOR BID). ACTUAL LOCATIONS TO BE COORDINATED WITH ENGINEER IN THE FIELD.
5. FINISHED GRADE DESIGN INTENDS TO UTILIZE PULVERIZED MATERIAL IN SHAPING AND PREPARATION OF THE ROADWAY. ROADWAY EXCAVATION QUANTITY IDENTIFIES EXPECTED EXPORT. IMPORT FOR SUBGRADE IS NOT ANTICIPATED.
6. PULVERIZATION DEPTH = 8". REMOVE EXCESS PULVERIZED MATERIAL AS NECESSARY TO PROVIDE SHOWN HMA SECTION.
7. HMA FINISH SURFACE SHALL BE 1/4-INCH ABOVE GUTTER LIP AFTER FINAL COMPACTION, FOR BOTH EXISTING CURB AND GUTTER AND REPLACED. LONGITUDINAL JOINT SEALER SHALL BE APPLIED AT ALL JOINTS BETWEEN GUTTER AND HMA.
8. COMPACTION SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 2-03.3(14)C. METHOD B.
9. THIS SECTION IS TO BE CONSTRUCTED WHERE SLOPED SECTION IS NOT SPECIFIED.

**A 06 TYPICAL DIVISION ST. AND TONASKET AVE. CROWNED SECTION**

N.T.S.

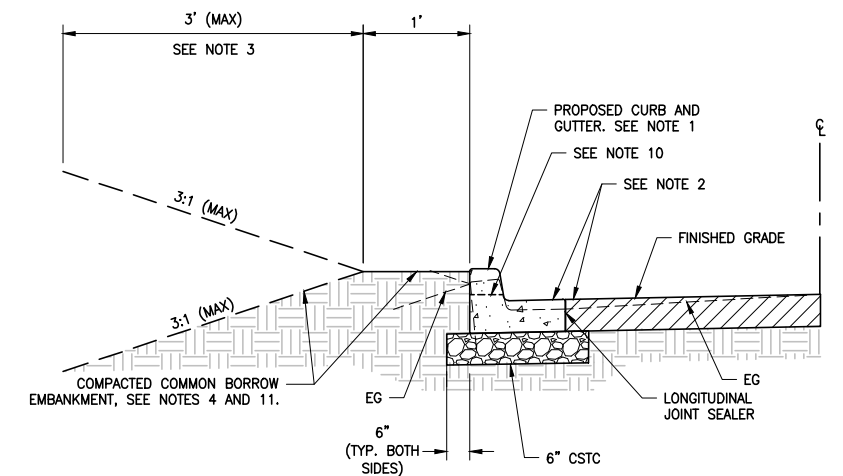


**NOTES**

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2. MATCH EXISTING LANE WIDTHS.
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4. SEE PLAN VIEW FOR ANTICIPATED CURB AND GUTTER REPLACEMENT LOCATIONS (USED FOR BID). ACTUAL LOCATIONS TO BE COORDINATED WITH ENGINEER IN THE FIELD.
5. FINISHED GRADE DESIGN INTENDS TO UTILIZE PULVERIZED MATERIAL IN SHAPING AND PREPARATION OF THE ROADWAY. ROADWAY EXCAVATION QUANTITY IDENTIFIES EXPECTED EXPORT. IMPORT FOR SUBGRADE IS NOT ANTICIPATED.
6. PULVERIZATION DEPTH = 8". REMOVE EXCESS PULVERIZED MATERIAL AS NECESSARY TO PROVIDE SHOWN HMA SECTION.
7. HMA FINISH SURFACE SHALL BE 1/4-INCH ABOVE GUTTER LIP AFTER FINAL COMPACTION, FOR BOTH EXISTING CURB AND GUTTER AND REPLACED. LONGITUDINAL JOINT SEALER SHALL BE APPLIED AT ALL JOINTS BETWEEN GUTTER AND HMA.
8. COMPACTION SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 2-03.3(14)C. METHOD B.

**B 06 SLOPED (SUPER-ELEVATED) TONASKET AVE. SECTION**

N.T.S.



**NOTES**

1. REMOVE EXISTING CURBING. CONTRACTOR TO MARK EXTENTS OF EXISTING CURB AND GUTTER REMOVAL FOR APPROVAL BY ENGINEER PRIOR TO SAWCUTTING/REMOVING. (TYP.)
2. THE DESIGN INTENDS TO REPLACE BROKEN, SUNKEN, AND/OR OVERTURNED SECTIONS OF EXISTING CURBING AND ADJACENT PAVEMENT. CONTRACTOR TO MAINTAIN UNIFORM ROAD SECTIONS SHOWN BETWEEN PORTIONS OF EXISTING CURBING TO REMAIN. FINISHED PROFILE TO BE SMOOTH AND FREE OF LOCAL SINKS OR ABRUPT GRADE CHANGES. UTILIZE PULVERIZED MATERIAL TO SHAPE ROAD ACCORDINGLY.
3. DAYLIGHT WITHIN 3.0', UNLESS SHOWN OTHERWISE OR AS DIRECTED BY ENGINEER IN THE FIELD.
4. HYDROSEED ALL DISTURBED AREAS. (TYP.)
5. CONSTRUCT TRAFFIC CURB AND GUTTER IN ACCORDANCE WITH WSDOT STANDARD PLAN F-10.12-03.
6. CURB EXPANSION JOINTS SHALL BE IN ACCORDANCE WITH WSDOT STANDARD PLAN F-30.10-03. SPACING = 12'.
7. SUB-GRADE MATERIAL MAY COME FROM INSITU SOILS THAT ARE APPROVED BY THE ENGINEER.
8. COMPACTION SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 2-03.3(14)C. METHOD B.
9. CURB SUBGRADE EXCAVATION SHALL BE INCLUDED IN THE PRICE OF THE CURB.
10. PROVIDE CURB DROPS AT EXISTING CURB DROP LOCATIONS. CURB HEIGHT BETWEEN DROPS TO BE 1". SEE PLAN VIEW FOR MORE INFORMATION.
11. WHERE SHOWN, PROVIDE CSTC DRIVEWAY INSTEAD OF COMMON BORROW EMBANKMENT. SEE PLANS.

**C 06 DETAIL - CURB AND GUTTER SECTION IN LOCATIONS OF CURB AND GUTTER REPLACEMENT**

N.T.S.

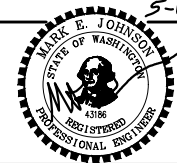


Know what's below. Call before you dig.

ONE INCH AT FULL SIZE IF NOT ONE INCH SCALE ACCORDINGLY

582603 Sht 06 (Details)

5-01-20



SCALE: AS SHOWN  
 DESIGNED: VAC/MEJ  
 DRAWN: VAC/TVP  
 CHECKED:  
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**VARELA AND ASSOCIATES, INC.**  
 ENGINEERING AND MANAGEMENT

**CITY OF TONASKET, WASHINGTON**  
 TONASKET AVE. & DIVISION ST. REHABILITATION PROJECT

TYPICAL ROAD SECTIONS

SHEET  
**06**  
 OF 07

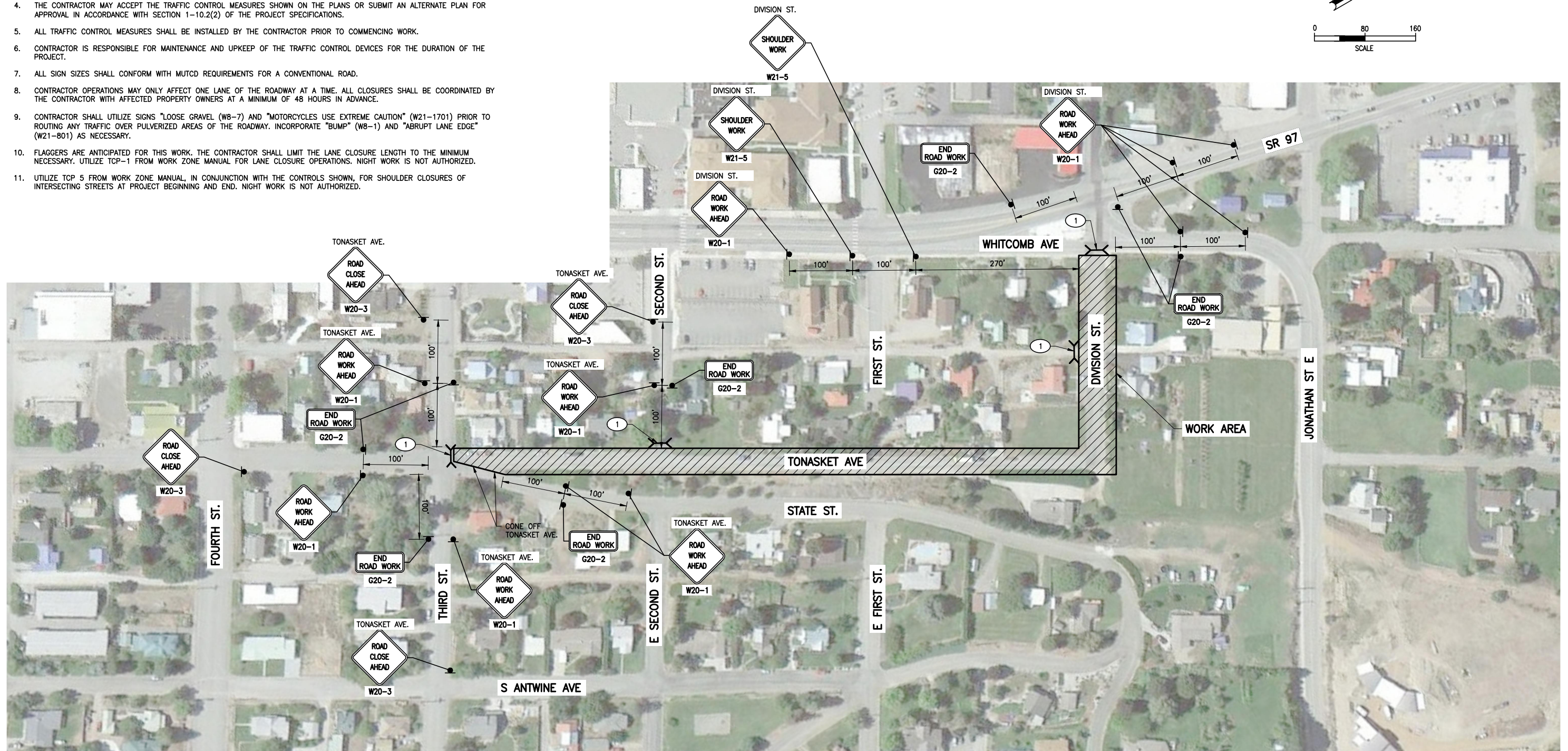
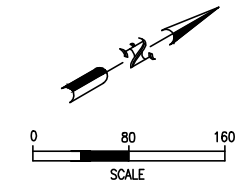
NO.	DATE	BY	CKD.	APP.	REVISIONS

**TRAFFIC CONTROL NOTES**

- ALL LABOR, MATERIAL, AND EQUIPMENT REQUIRED TO IMPLEMENT AND MAINTAIN A TRAFFIC CONTROL PLAN MEETING THE REQUIREMENTS HEREIN AND AS REQUIRED TO CONTROL TRAFFIC IN ACCORDANCE WITH ALL APPLICABLE REGULATIONS FOR THE DURATION OF THE PROJECT SHALL BE INCLUDED IN THE PROJECT TEMPORARY TRAFFIC CONTROL BID ITEM.
- CONTRACTOR TO PROVIDE ALL TRAFFIC CONTROLS AND TEMPORARY RAMPS TO ALLOW LOCAL RESIDENTS ACCESS TO THEIR PROPERTY AT ALL TIMES.
- TRAFFIC CONTROL PRACTICES AND DEVICES SHALL CONFORM WITH THE STANDARDS OF THE 2009 MUTCD AND THE WSDOT 2018 WORK ZONE TRAFFIC CONTROL GUIDELINES FOR MAINTENANCE OPERATIONS.
- THE CONTRACTOR MAY ACCEPT THE TRAFFIC CONTROL MEASURES SHOWN ON THE PLANS OR SUBMIT AN ALTERNATE PLAN FOR APPROVAL IN ACCORDANCE WITH SECTION 1-10.2(2) OF THE PROJECT SPECIFICATIONS.
- ALL TRAFFIC CONTROL MEASURES SHALL BE INSTALLED BY THE CONTRACTOR PRIOR TO COMMENCING WORK.
- CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE AND UPKEEP OF THE TRAFFIC CONTROL DEVICES FOR THE DURATION OF THE PROJECT.
- ALL SIGN SIZES SHALL CONFORM WITH MUTCD REQUIREMENTS FOR A CONVENTIONAL ROAD.
- CONTRACTOR OPERATIONS MAY ONLY AFFECT ONE LANE OF THE ROADWAY AT A TIME. ALL CLOSURES SHALL BE COORDINATED BY THE CONTRACTOR WITH AFFECTED PROPERTY OWNERS AT A MINIMUM OF 48 HOURS IN ADVANCE.
- CONTRACTOR SHALL UTILIZE SIGNS "LOOSE GRAVEL (W8-7) AND "MOTORCYCLES USE EXTREME CAUTION" (W21-1701) PRIOR TO ROUTING ANY TRAFFIC OVER PULVERIZED AREAS OF THE ROADWAY. INCORPORATE "BUMP" (W8-1) AND "ABRUPT LANE EDGE" (W21-801) AS NECESSARY.
- FLAGGERS ARE ANTICIPATED FOR THIS WORK. THE CONTRACTOR SHALL LIMIT THE LANE CLOSURE LENGTH TO THE MINIMUM NECESSARY. UTILIZE TCP-1 FROM WORK ZONE MANUAL FOR LANE CLOSURE OPERATIONS. NIGHT WORK IS NOT AUTHORIZED.
- UTILIZE TCP 5 FROM WORK ZONE MANUAL, IN CONJUNCTION WITH THE CONTROLS SHOWN, FOR SHOULDER CLOSURES OF INTERSECTING STREETS AT PROJECT BEGINNING AND END. NIGHT WORK IS NOT AUTHORIZED.

**KEY NOTES**

- TYPE III BARRICADE WITH WARNING LIGHTS AND ROAD CLOSED TO THRU TRAFFIC (R11-4)



582603 Sht 07 (Traffic Control)



Know what's below.  
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5-01-20

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TONASKET AVE. & DIVISION ST. REHABILITATION PROJECT

TRAFFIC CONTROL PLAN

SHEET  
**07**  
OF 07

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